



## **Appendix C**

### **Public Involvement and Agency Coordination**

#### **C2 - PUBLIC INVOLVEMENT MEETINGS**

##### **PART 4 – OCTOBER 27, 2011 Meeting Appendices**

#### **APPENDICES**

Appendix C – Exhibits (a)



# CREATE Program Map



**Project Type**

- Rail Improvement
- Highway-Rail Grade Separation
- ▲ Passenger-Specific Project
- Environmentally Linked Projects

**CREATE Designated Corridors**

- P Passenger Corridors
- EW East-West Corridor
- B Beltway Corridor
- WA Western Ave. Corridor

Note: The first letter of a project indicates its Corridor. "T" projects are rail operations Towers. "GS" projects are Grade Separations.

## Goals

- Reduce rail and roadway congestion
- Improve passenger and freight rail service

## Benefits

- Enhance public safety
- Provide national, regional and local economic benefits
- Reduce energy consumption
- Improve regional air quality



## Project Purpose

To improve mobility for rail passengers, freight, and motorists

## Project Needs

- Reduce rail-rail crossing conflicts
  - Forest Hill Junction
  - 80<sup>th</sup> Street Junction
  - Metra SouthWest Service connection to Metra Rock Island
  - Belt Junction
- Reduce road-rail crossing conflicts
  - 71<sup>st</sup> Street crossing
- Improve rail passenger service reliability
  - Along Columbus Avenue
- Reduce local mobility problems
  - Viaducts





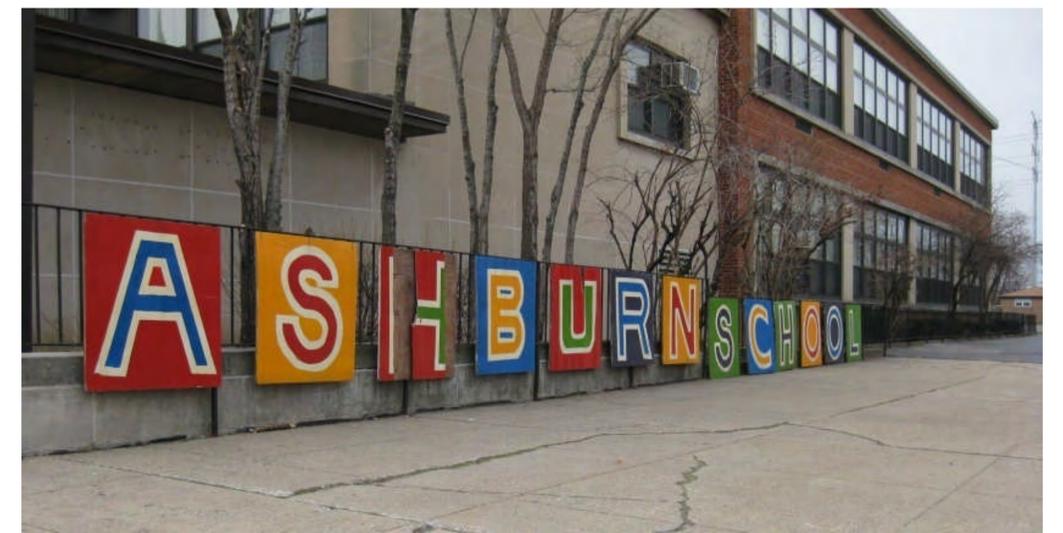
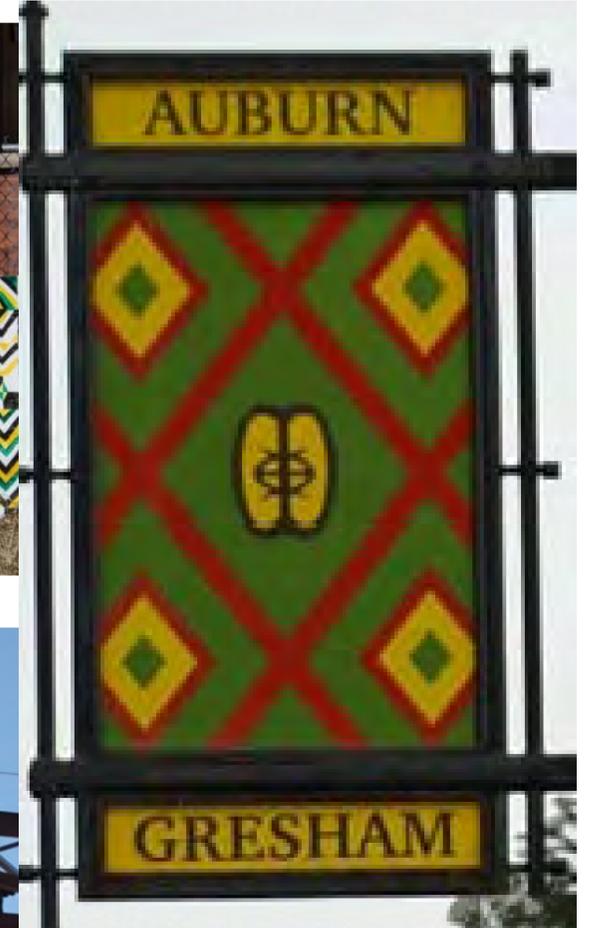
The 75<sup>th</sup> St. CIP Team is preparing an **Environmental Impact Statement (EIS)**, the highest level of federal environmental review. The EIS will:

- Explain why the project is needed: the “Purpose and Need” of the project
- Evaluate reasonable Alternatives
- Describe the potential impacts and benefits from the range of reasonable Alternatives
- Identify the Preferred Alternative that best balances benefits and impacts
- Describe in detail how the decision was made
- Explain how the public was engaged throughout the process

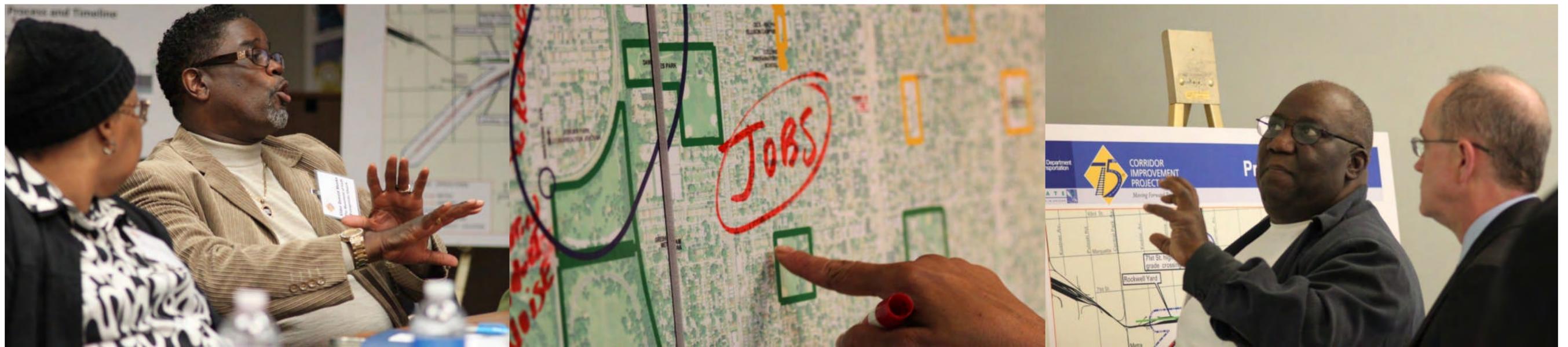


IDOT is using the **Context Sensitive Solutions** process on this project to:

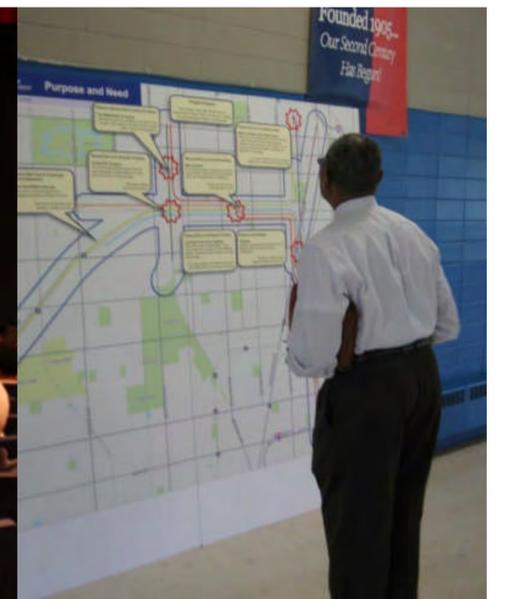
- Work with your community to find transportation solutions that work in coordination with your surroundings
- Engage all stakeholders
- Balance the project's transportation needs with your community's concerns and values



- April:** Introduced the project, asked for input on draft statement of transportation problems, and used that input to develop Purpose and Need
- August and September:** Presented and gathered input on Alternates developed to address transportation problems
- September:** Presented viaduct inspection results and cost estimates



- Held Public Meetings on June 7 and 9, 2011
- Introduced the project
- Reviewed 75<sup>th</sup> St. CIP draft Purpose and Need
- Obtained public input on 75<sup>th</sup> St. CIP and the draft Purpose and Need



Any property acquisition will follow the federal Uniform Act. The act provides for:

- Fair compensation
- Relocation assistance and moving expenses
- Other costs associated with comparable replacement housing
- Opportunity to relocate within the community if possible



The acquisition of property in the neighborhood south of Hamilton Park is likely. As part of this EIS study we will:

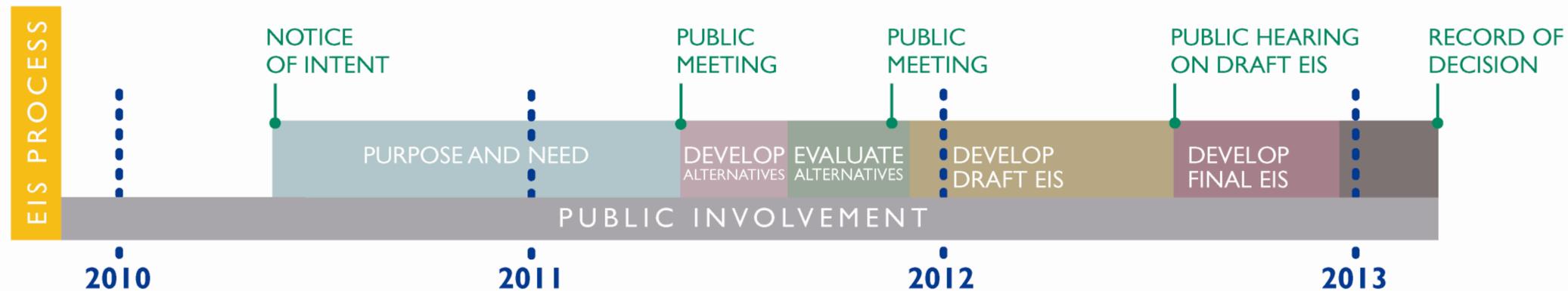
- Minimize the need to acquire property
- Present the potential impacts of each alternative to the community at this meeting
- Define the exact number and location of any acquisitions
- Notify all owners and occupants of property adjacent to or potentially affected by the project of public meetings by mail.



## Timing of acquisitions:

- The property acquisition process can begin after the Federal Highway Administration approves the study and signs a Record of Decision (ROD). The current schedule anticipates a ROD in **March 2013**.
- The process will allow time for an orderly relocation.

Property acquisition process can begin



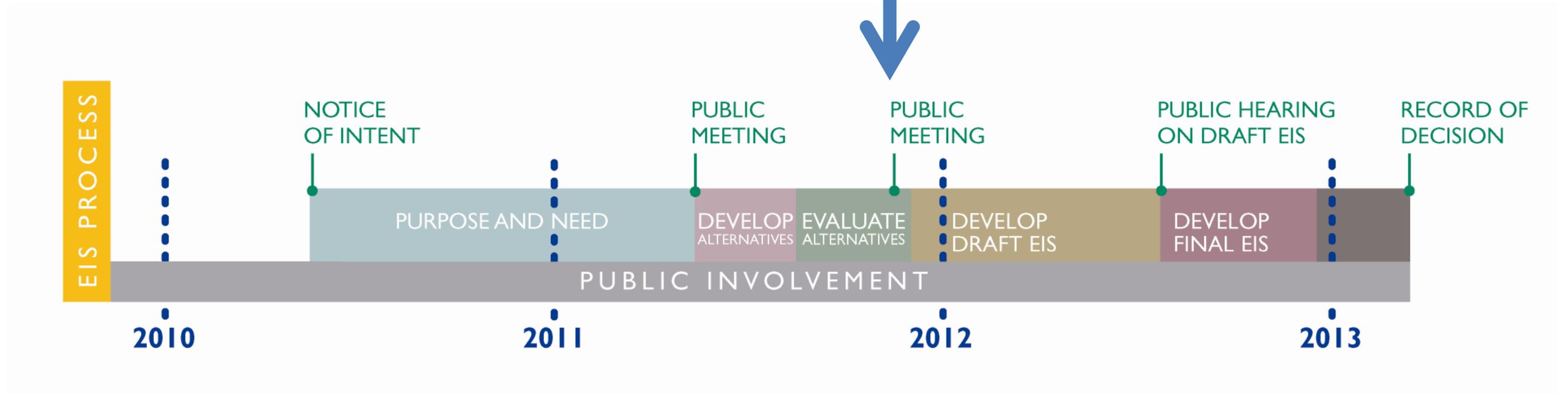


- Submit comments today
- Attend public hearing in 2012
- Request speakers for local group meetings
- Read brochures and other project publications
- Send us comments through:
  - Online comment form at [www.75thCIP.org](http://www.75thCIP.org)
  - Project team email – [info@75thCIP.org](mailto:info@75thCIP.org)
  - Mail – One North Franklin, Ste. 500  
Chicago, IL 60606  
Attn: Gretchen Wahl



# Environmental Impact Statement Process and Timeline

We Are Here



**Junction**—The point where two or more rail lines cross and/or converge. The railroad equivalent of an intersection.

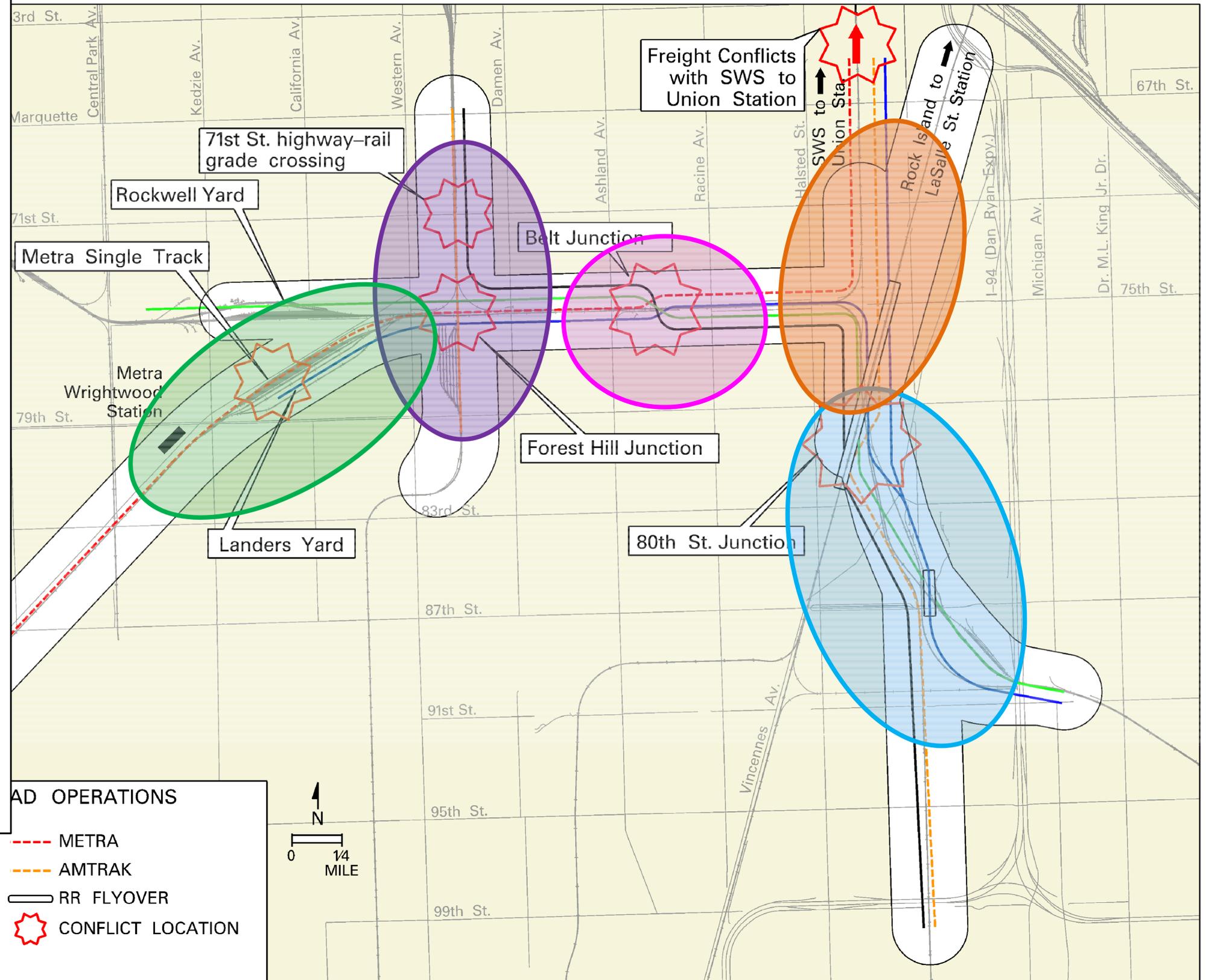
**Diamond Crossing**—A type of junction where two rail lines cross at the same level, forming a diamond.

**Grade Separation**—An underpass or an overpass of a highway and a railroad (often referred to as a viaduct).



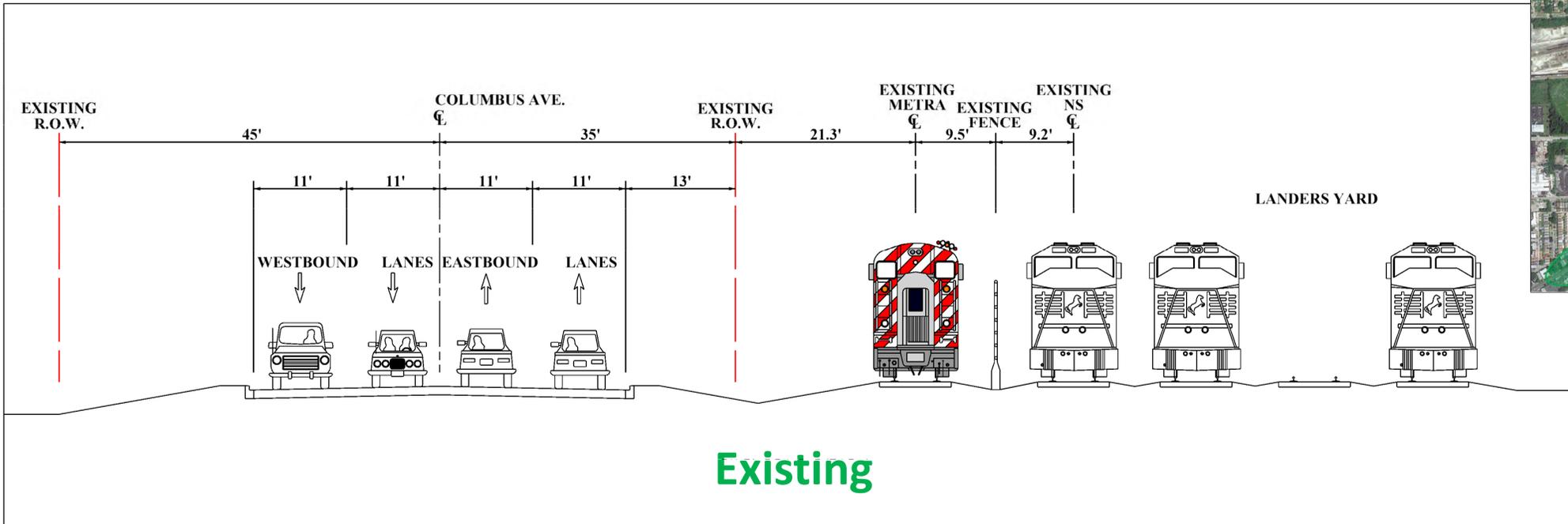


- **Metra Reliability Columbus Ave.**
- **Forest Hill Junction & 71<sup>st</sup> Street**
- **Belt Junction**
- **Metra Rock Island Connection**
- **80<sup>th</sup> Street Junction**
- **Local Mobility (Viaducts)**

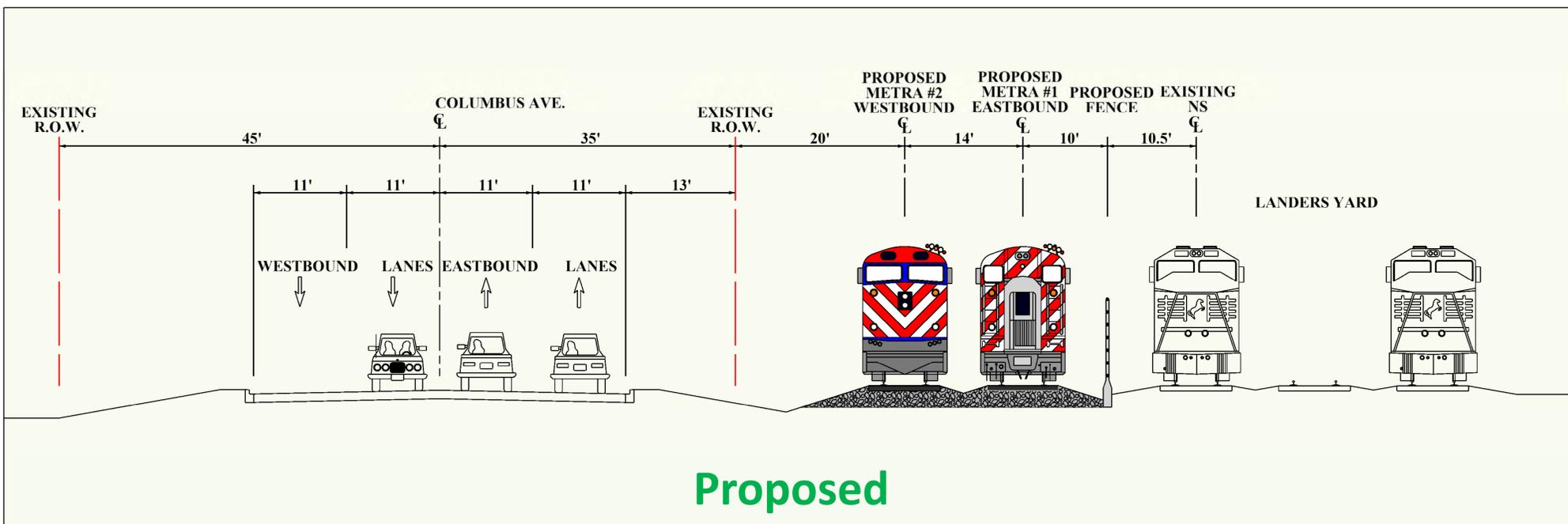




# Improve Metra Reliability

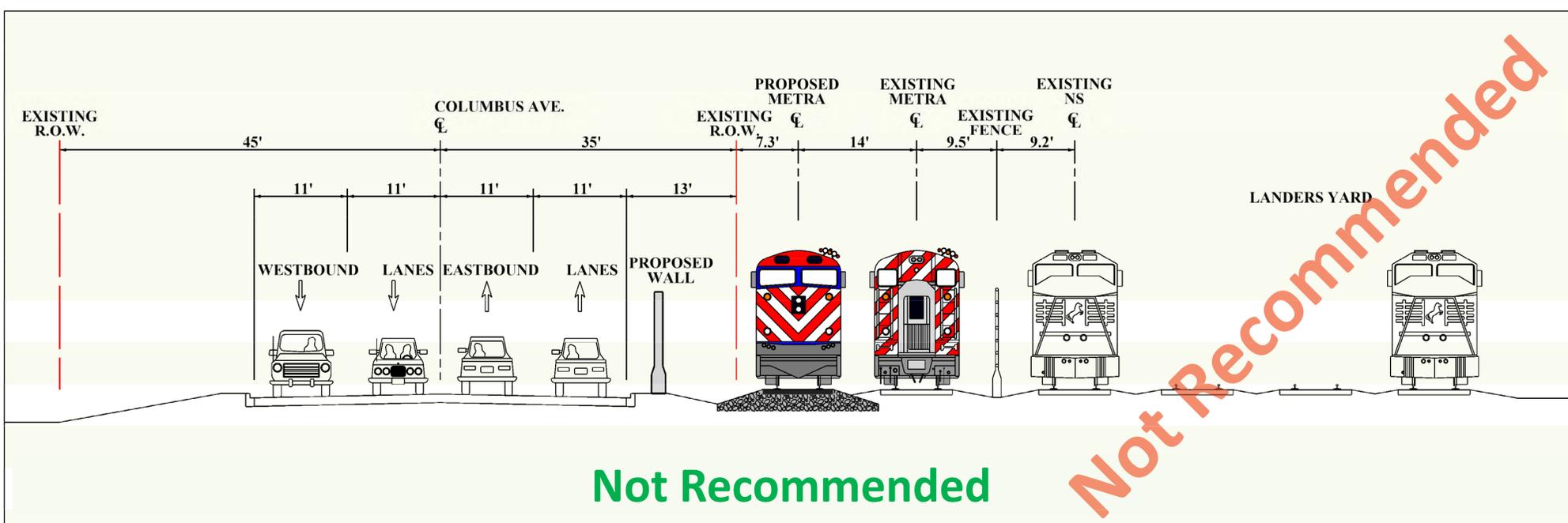


A



**2<sup>nd</sup> track replaces Landers yard track**

- Track one foot closer to Columbus Ave.
- Train headlights 1' closer to traffic
- Room for road and track maintenance



**2nd track closer to Columbus Ave.**

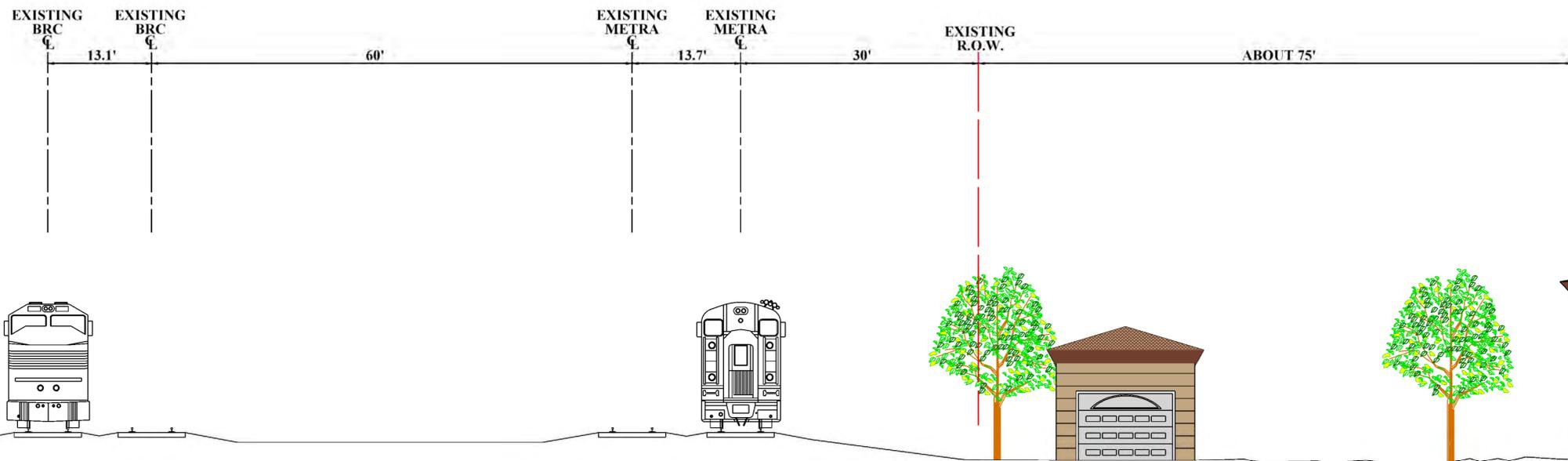
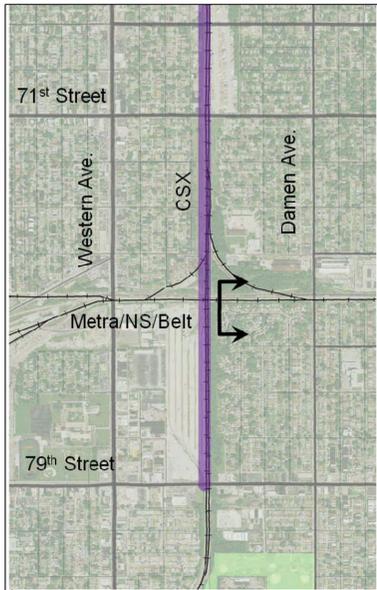
- Track 14 feet closer to Columbus Ave.
- Train headlights much closer to traffic
- No room for road and track maintenance

*Not Recommended*

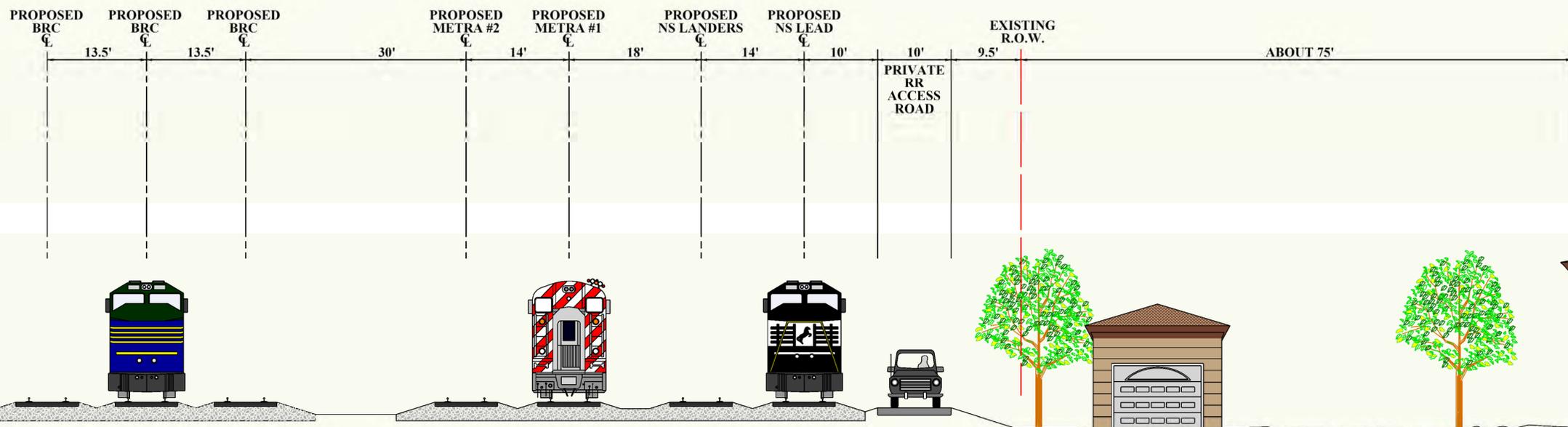




## CSX over Metra, BRC, & NS – 75<sup>th</sup> Street Tracks (facing east)



### Existing



### Proposed



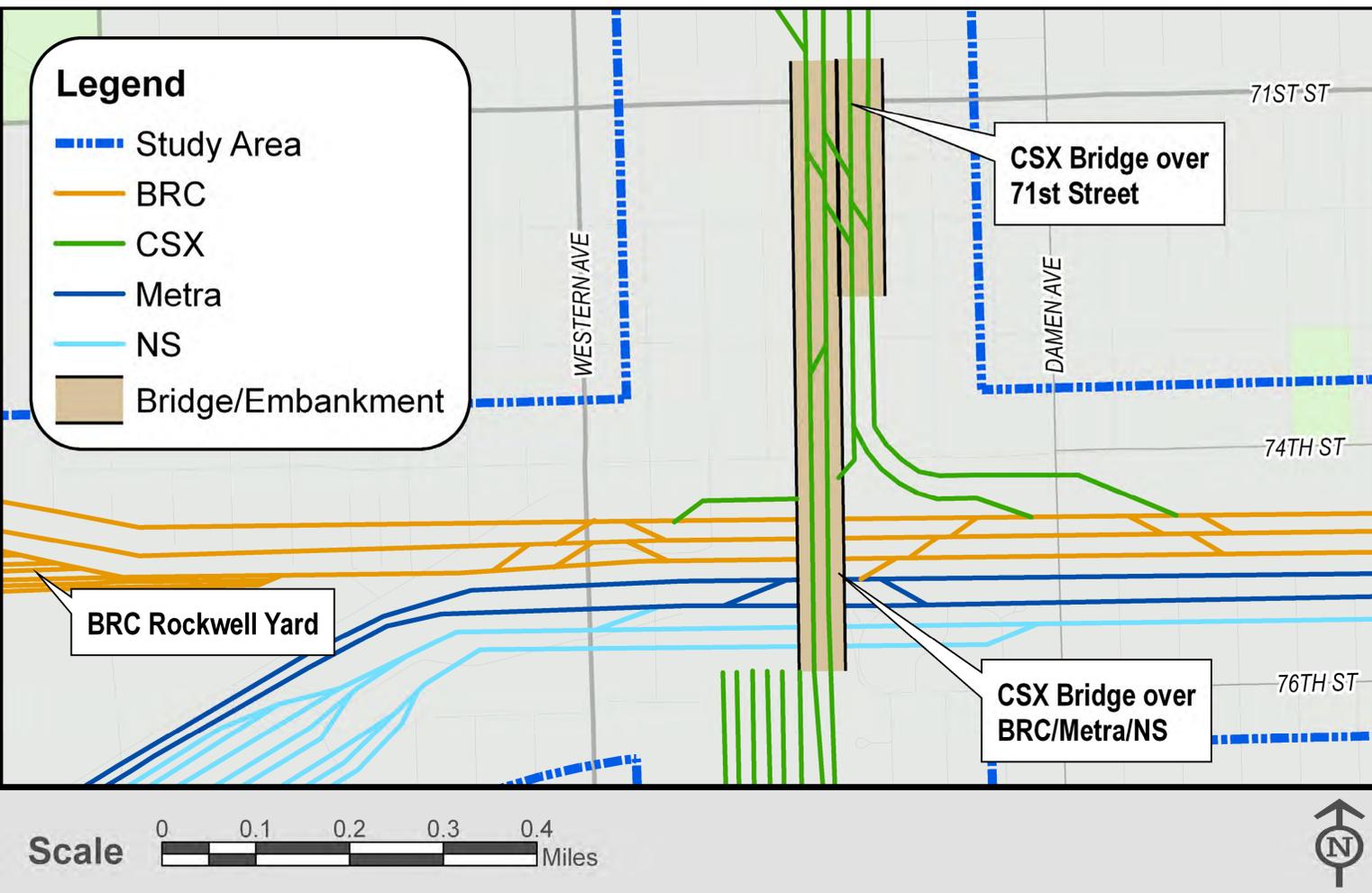


# Forest Hill and 71<sup>st</sup> Street

## Artist's rendering of proposed CSX tracks – Hamilton Avenue north of 76<sup>th</sup> Street



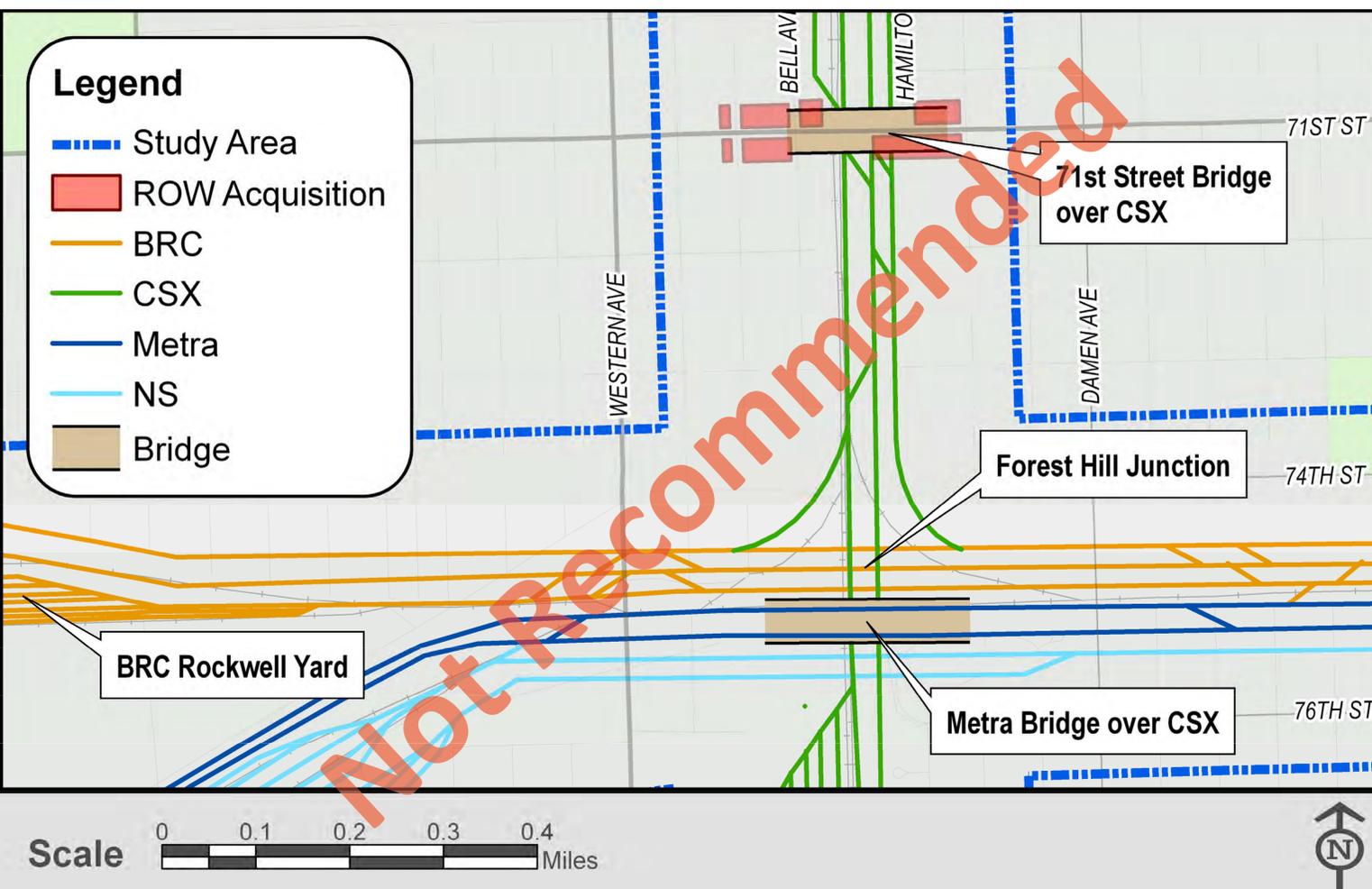
## CSX over Metra, BRC, & NS



### CSX over railroads and 71<sup>st</sup> St.

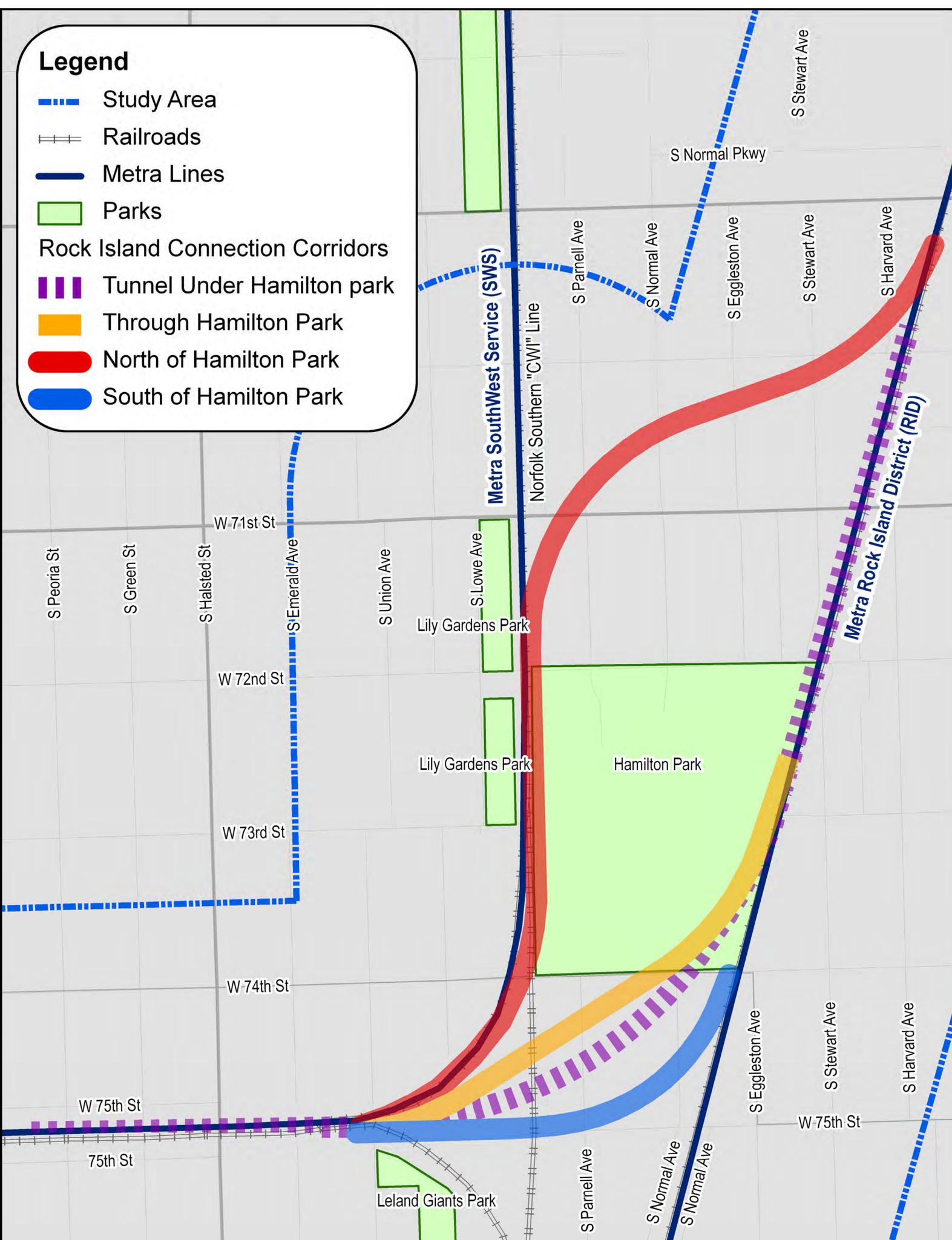
- Eliminates all rail conflicts at Forest Hill Junction
- Eliminates noise and pollution from waiting trains at Forest Hill Junction
- Eliminates rail-rail conflicts at 71<sup>st</sup> St.
- No property acquisition
- Access to 71<sup>st</sup> St. from Bell and Hamilton remains open
- All noise from diamond crossings eliminated

## Metra bridge over CSX, 71<sup>st</sup> Street over CSX



### Metra over CSX; 71<sup>st</sup> St. over CSX

- Eliminates Metra-freight conflicts at Forest Hill Junction, but not freight-freight conflicts
- Reduces noise and pollution from waiting trains at Forest Hill Junction
- Eliminates rail-rail conflicts at 71<sup>st</sup> St.
- 48 single family homes and one business would be acquired
- Access to 71<sup>st</sup> St. would be eliminated at Bell and Hamilton
- Noise from Forest Hill Junction diamond crossing would remain



## South of Hamilton Park

- Shortest connection
- Acquisition of about 20 residential properties
- Least expensive of permitted Alternates

## North of Hamilton Park

- Longest connection and bridge
- Acquisition of about 60 residential properties
- Very expensive

## Through Hamilton Park

- Least residential property acquisition
- Park protected by federal law

*Not Recommended*

## Tunnel under Hamilton Park

- Property acquisition above tunnel
- 5 streets closed in approach to tunnel
- Large ventilation shafts
- Most expensive

*Not Recommended*



# Metra Rock Island Connection

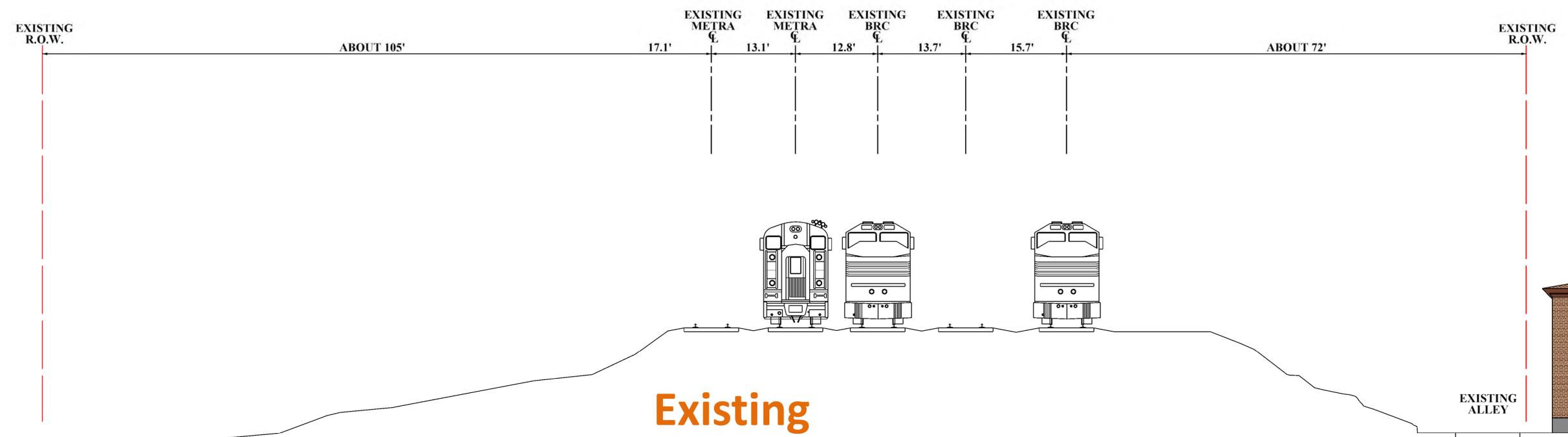
## Artist's rendering of flyover – Parnell Avenue south of Hamilton Park



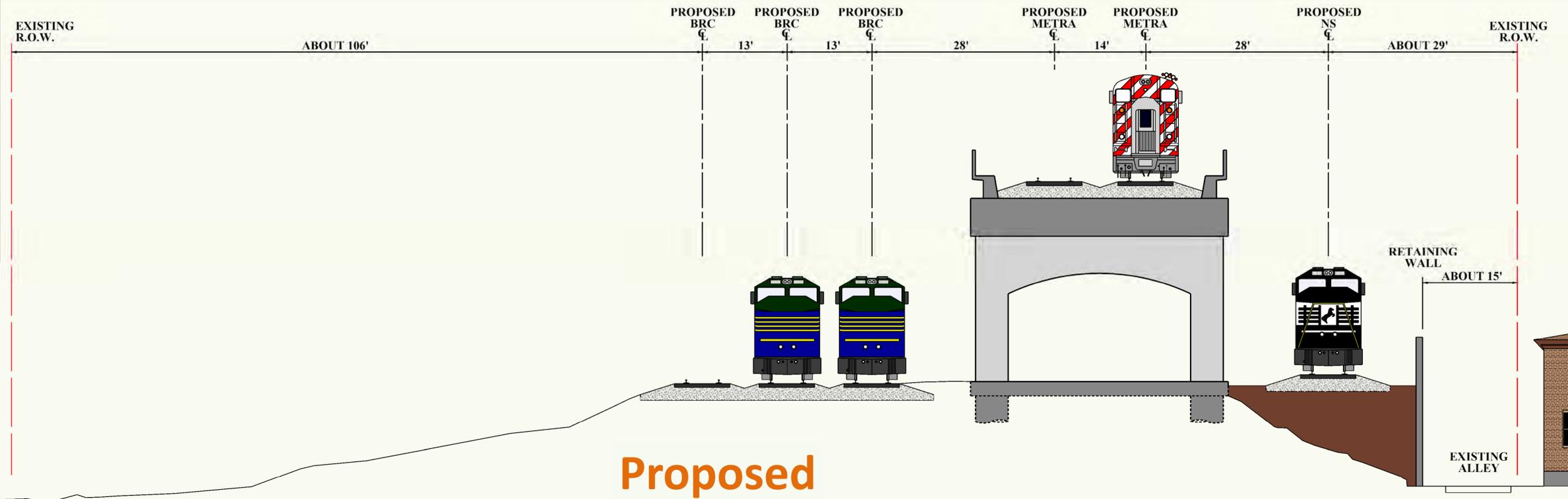
# Metra Rock Island Connection

## Near Emerald, facing east

A



Existing



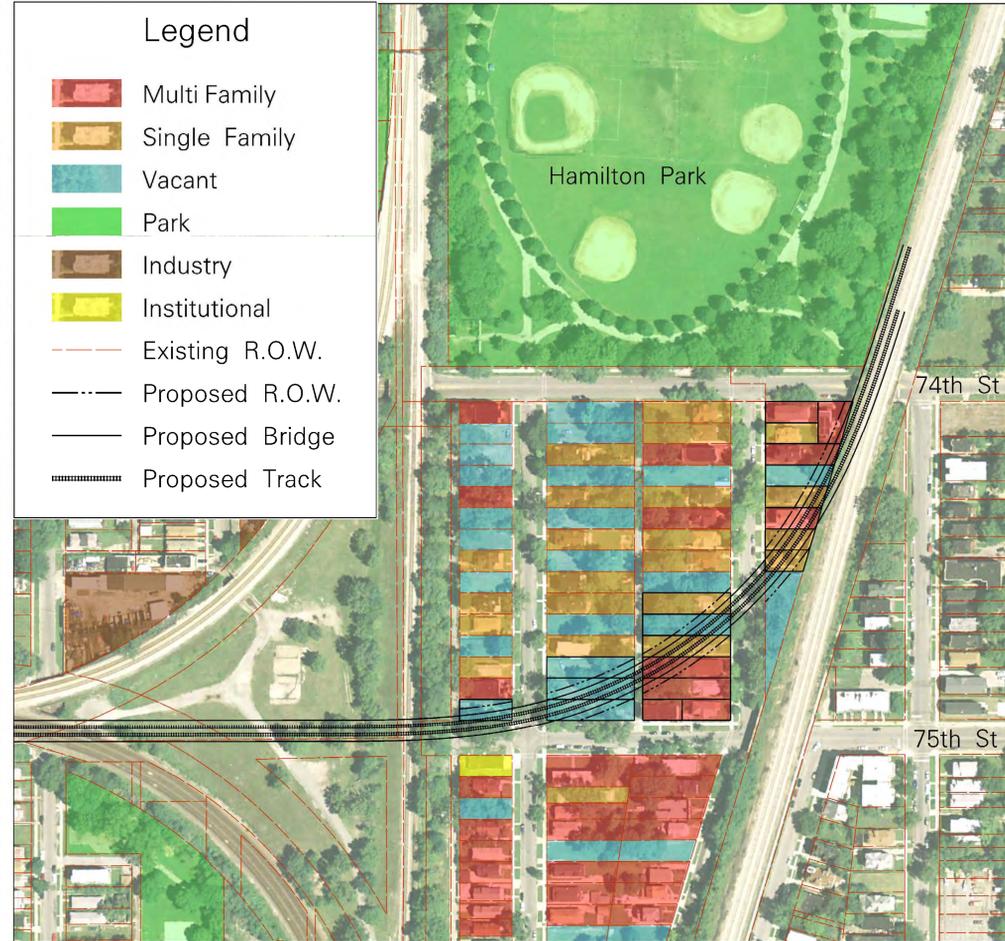
Proposed

# Metra Rock Island Connection

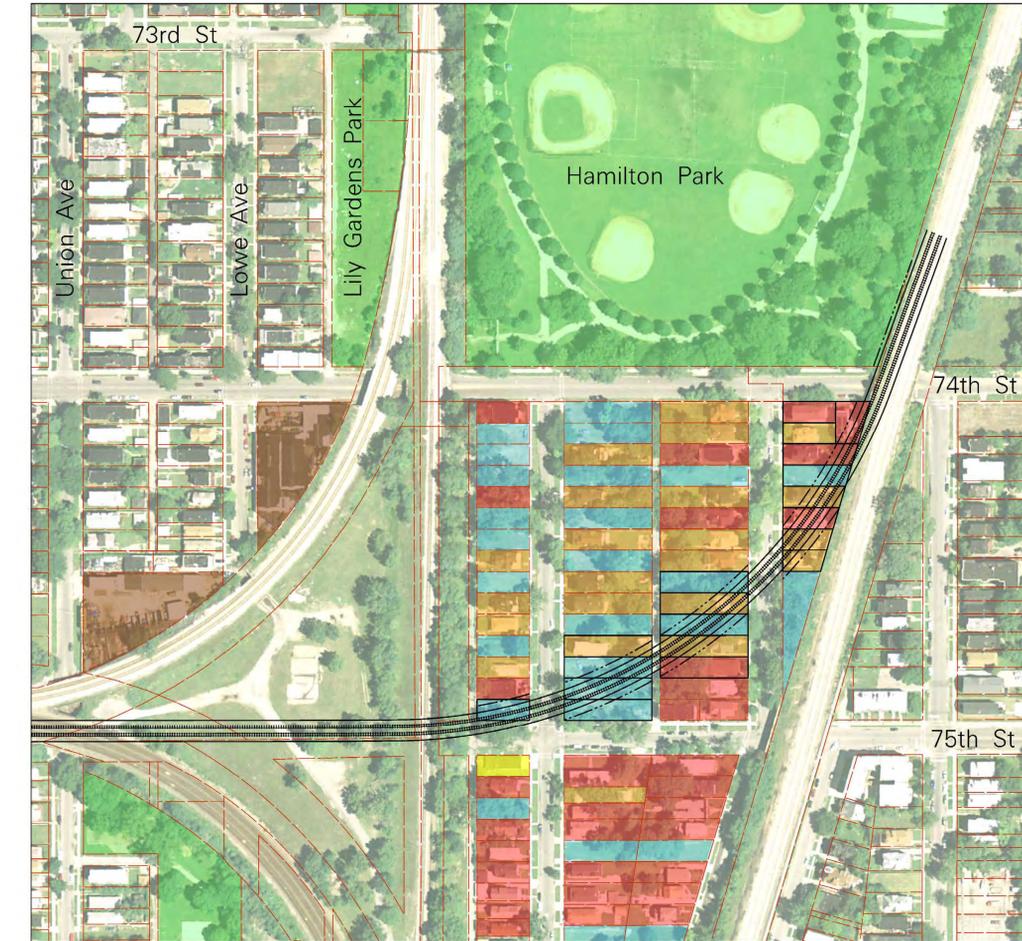
**Bridge South of Hamilton Park  
Alignment Alternate 1**



**Bridge South of Hamilton Park  
Alignment Alternate 2**



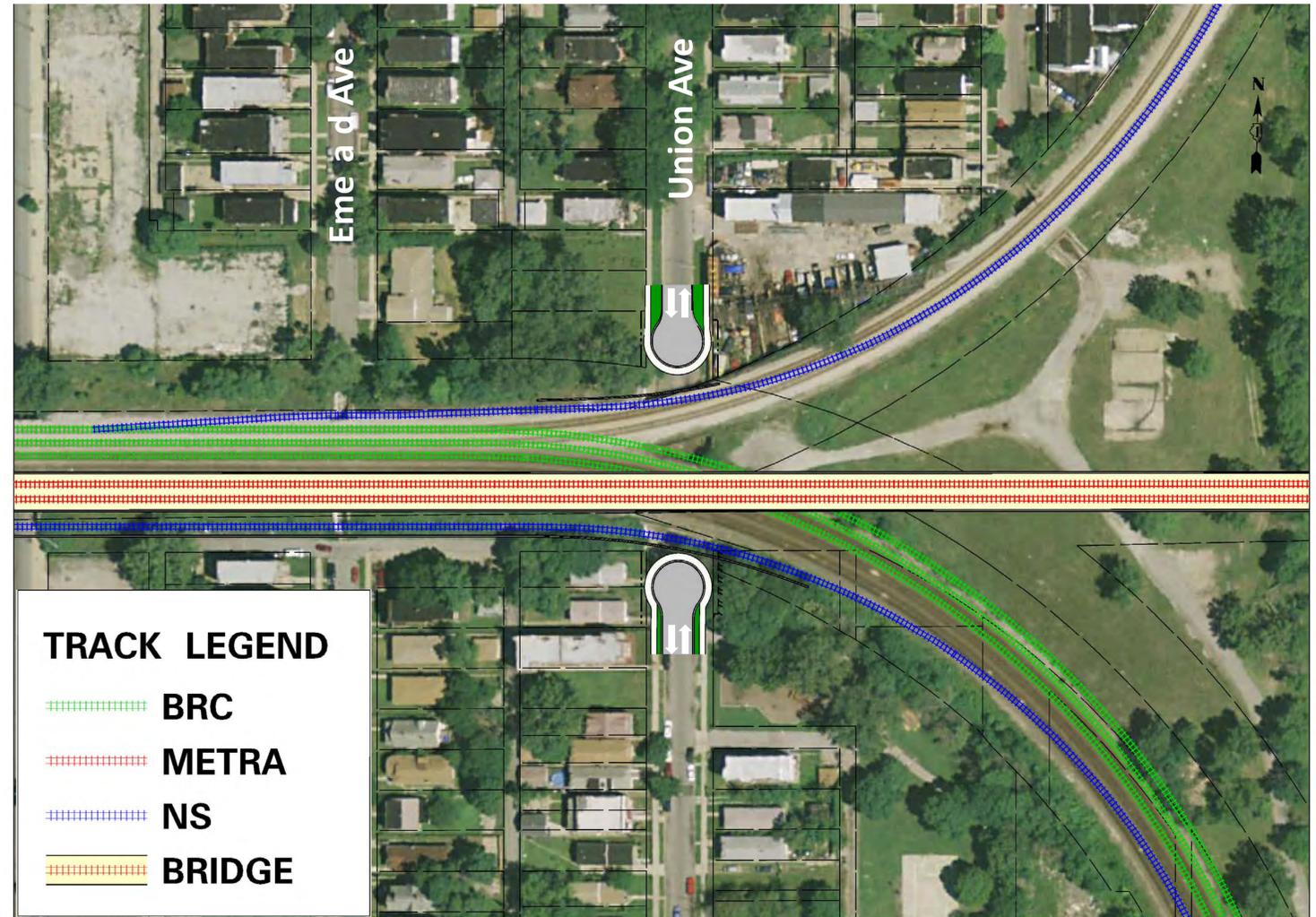
**Bridge South of Hamilton Park  
Alignment Alternate 3**



Alignment Alternate	Train Speed on Curve	Bridge Over 75th St	Park Land Needed	Properties Acquired				Dwelling Units
				Vacant Lots	Residential (single- and multi-unit)	Institutional (churches, etc.)	Total	
1	40	Yes	None	5	15	1	21	25
2	35	No	None	7	14	0	21	23
3	40	No	Minimal (0.032 acres)	8	12	0	20	18

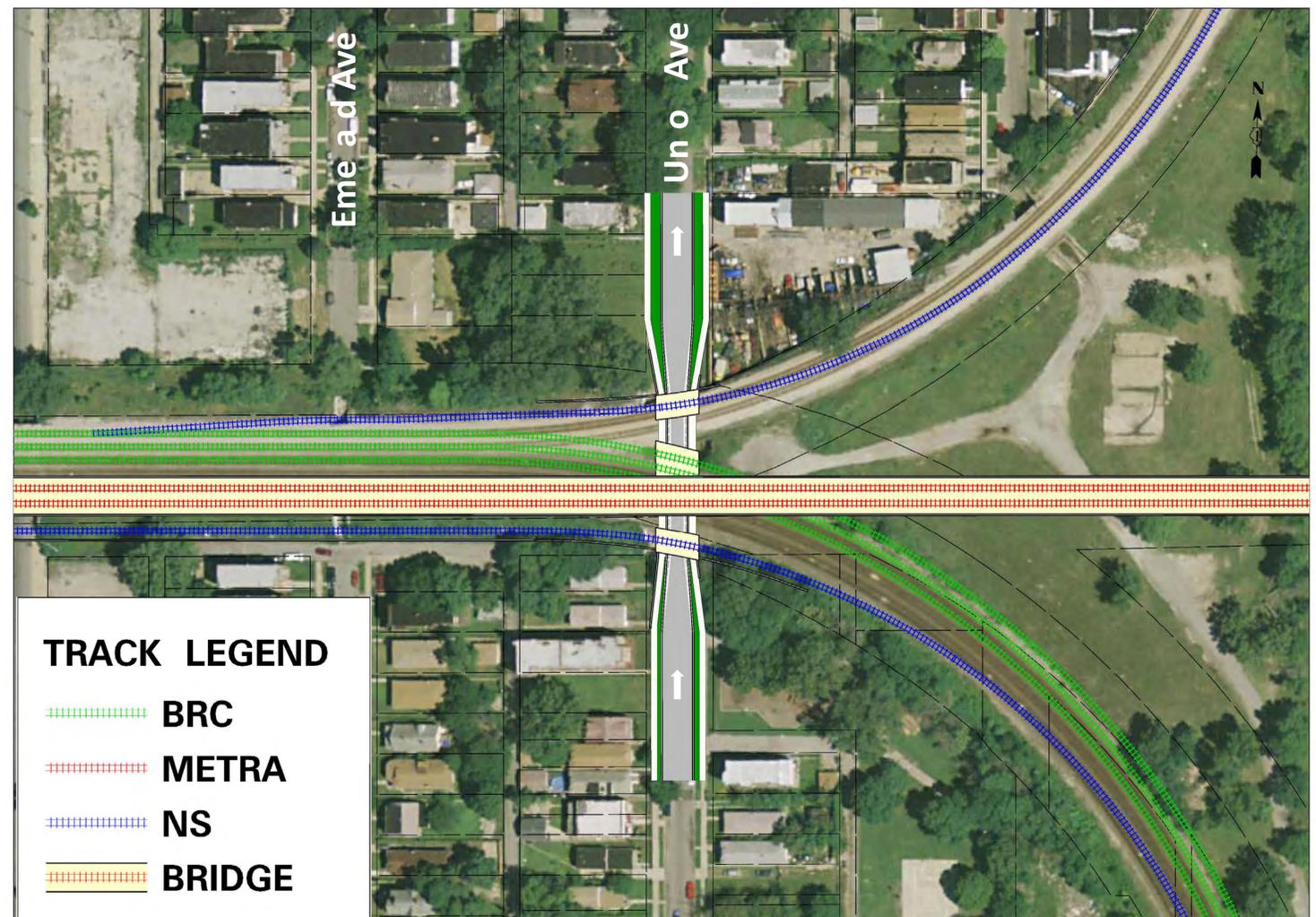
## Union Ave. Viaduct – Option 1

- Close bridge
- Cul-de-sac street on both sides of railroad
- Convert Union to two separate two-way streets



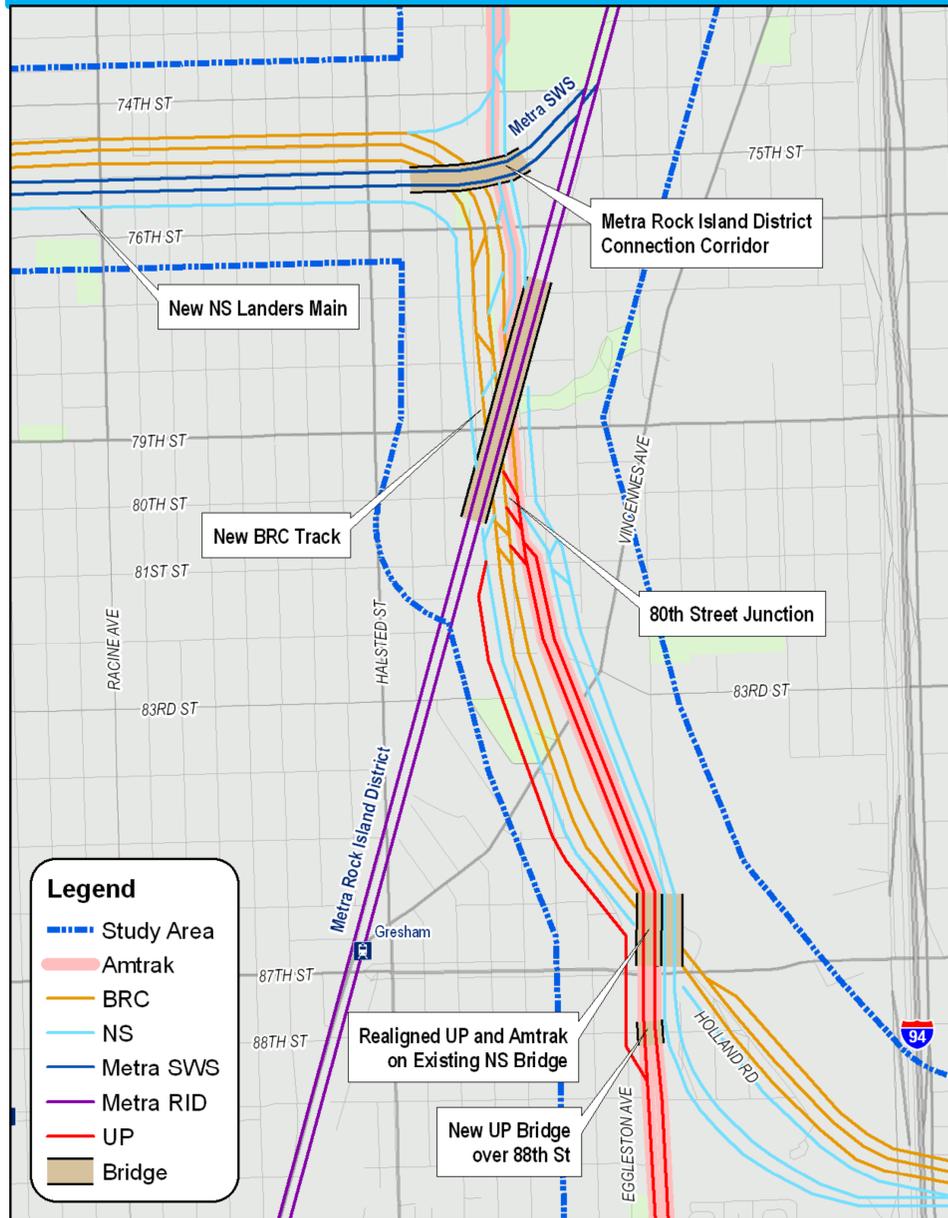
## Union Ave. Viaduct – Option 2

- Build new bridge
- Required clearance 14'6"
- Lower street 4 feet
- New sewer line to 74<sup>h</sup> or 76<sup>th</sup>
- Street closure during bridge construction (up to 1 year)



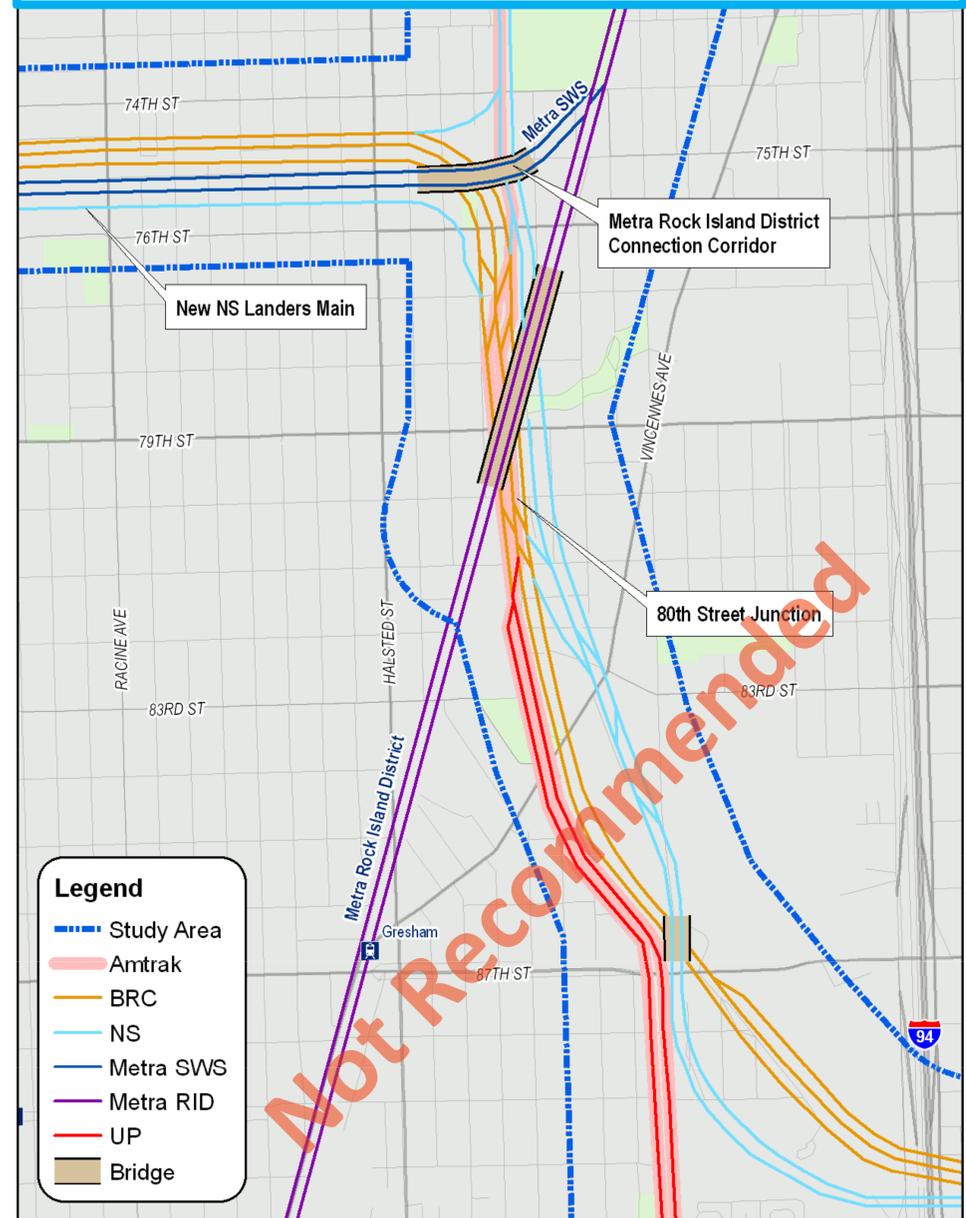


## Two additional tracks at 80<sup>th</sup> Street Use existing bridge

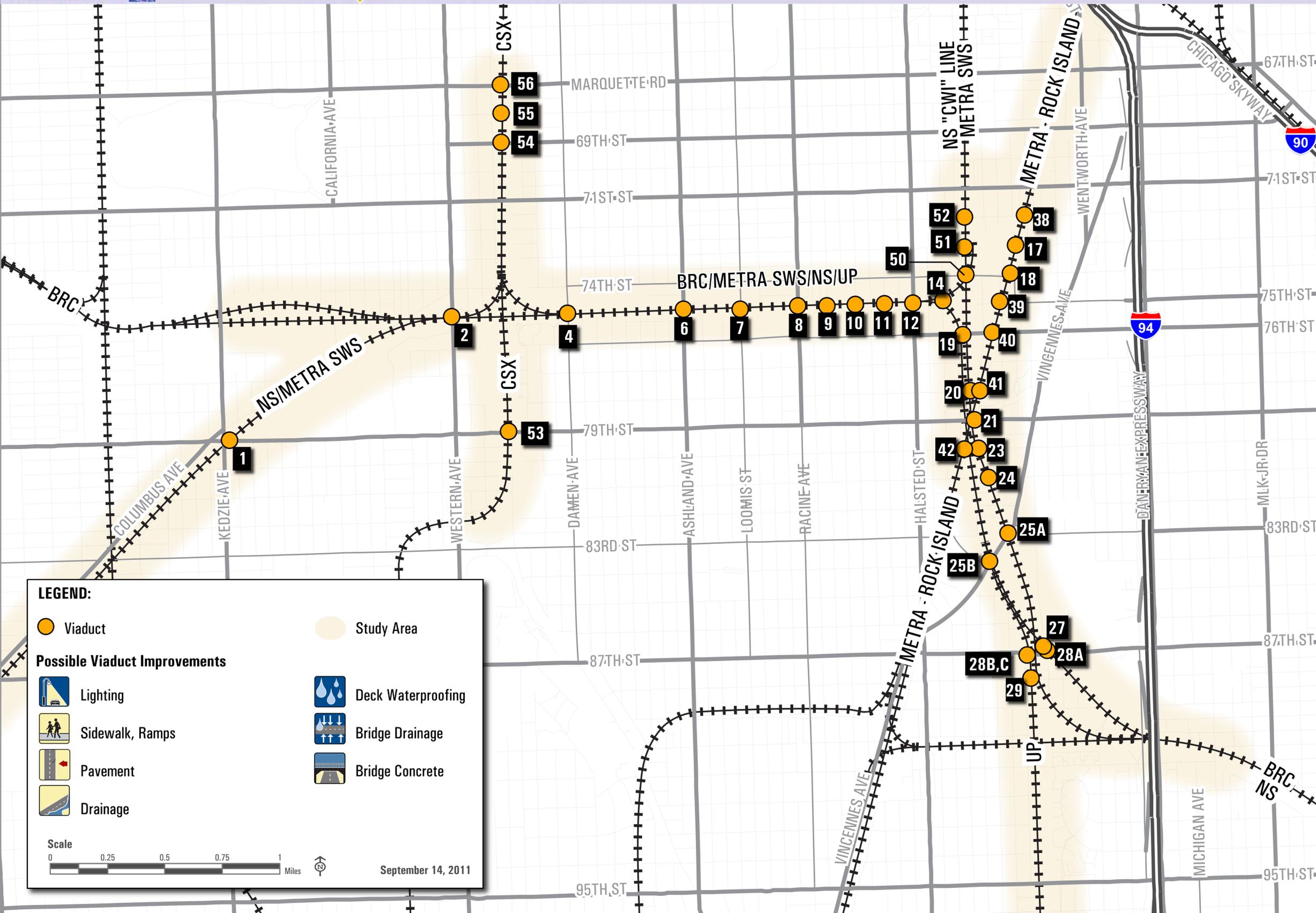


- Provides additional capacity at 80<sup>th</sup> Street
- Uses existing bridge to allow trains to cross the above other freight tracks
- Reduces conflicting movements at 80<sup>th</sup> Street
- Eliminates need for Belt Junction
- All work on existing railroad embankments
- No residential property acquisition

## Two additional tracks at 80<sup>th</sup> Street



- Provides additional capacity at 80<sup>th</sup> Street
- Trains must cross other freight tracks at grade near 80<sup>th</sup> Street
- Does not reduce conflicting movements at 80<sup>th</sup> Street
- Reduces need for Belt Junction
- All work on existing railroad embankments
- No residential property acquisition



**LEGEND:**

- Viaduct
- Study Area

**Possible Viaduct Improvements**

- Lighting
- Sidewalk, Ramps
- Pavement
- Drainage
- Deck Waterproofing
- Bridge Drainage
- Bridge Concrete

Scale: 0 0.25 0.5 0.75 1 Miles

September 14, 2011

Structure No.	75th St. CIP Project Eligible Work	Non-eligible Maintenance
1	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
2	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
4	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
6	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
7	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
8	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
9	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
10	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
11	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
12	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
14	Bridge to be Removed or Replaced	[Bridge Concrete]
17	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
18	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
19	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
20	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
21	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
23	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
24	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
25A	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
25B	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
27	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
28A	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
28B,C	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
29	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
38	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
39	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
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41	[Lighting, Sidewalk, Ramps, Pavement, Drainage, Deck Waterproofing]	[Bridge Concrete]
42	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
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54	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
55	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]
56	[Lighting, Sidewalk, Ramps, Pavement, Drainage]	[Bridge Concrete]

# Reduce Local Mobility Problems: Viaducts

## Inspected 36 project-area viaducts for:

- Lighting
- Drainage
- Roadway pavement
- Sidewalks and ramps
- Bridge façade concrete

## Identified repair types:

**Routine Maintenance** – Not eligible for 75<sup>th</sup> St. CIP funds.

**Replacement/Reconstruction** – Eligible for project funds. Can be part of 75<sup>th</sup> St. CIP.

## Reported maintenance needs to:

**City of Chicago** (lighting, roadway, drainage, sidewalks and ramps)

**Railroads** (bridge concrete)



## Maintenance Work

### The City of Chicago:

- Repaired 108 light fixtures at 26 viaducts throughout the 75<sup>th</sup> St. CIP project study area
- Cleared vegetation at the Hamilton Park pedestrian underpass at 73<sup>rd</sup> Street

